

East Bay Greenway Environmental Review and Implementation Strategy

North and Central County



PROJECT BENEFITS:

The East Bay Greenway will:

- Encompass transit-oriented developments—high-density, walkable communities centered around BART stations and mass transit lines like the AC Transit Rapid Bus Line.
- Provide mobility for economically and socially disadvantaged people through safe connections to five BART stations, two downtown areas, and multiple parks and schools.
- Eliminate barriers separating local communities.
- Encourage people to walk and cycle to stay healthy and reduce dependency on vehicles.

PROJECT STATUS:

Alameda CTC is working in collaboration with local and regional partners to implement the project, which is currently in the process of obtaining environmental clearance.

To find out more about the program, please contact the Project Sponsor.

PROJECT SPONSOR:

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Along a 12-mile stretch that begins in the City of Oakland and runs south toward the Hayward BART Station, the East Bay Greenway (the “Greenway”) project will transform areas beneath the elevated BART tracks into a landscaped, accessible bicycle and pedestrian trail. The Greenway will connect communities, offering residents healthier and safer modes of transportation between home, work and school destinations.

Running parallel to Interstates 580 and 880, the Greenway will cross five BART stations, six priority development areas and four jurisdictions—Oakland, San Leandro, unincorporated Alameda County, and Hayward—through some of the most densely populated and underrepresented areas in the region. More than 15 schools serving over 8,600 students in the area surround the 12-mile trail, along with small businesses, retail centers such as the Bayfair Mall in the City of San Leandro, medical facilities, community centers, industrial sites, and single-family and multi-family residential dwellings. The Greenway incorporates key objectives of Smart Mobility by integrating transportation and landuse in complementing the Priority Development Areas in the unincorporated areas of Alameda County and the Cities of Oakland, San Leandro, and Hayward. Additionally, it will improve critical pedestrian and bicycle friendly access to intermodal transit hubs like the Oakland Coliseum/Oakland Airport Amtrak and BART Stations and others along the East Bay BART line, as well as provide safer connections to AC Transit bus service routes.

The Greenway will provide a safe and accessible pedestrian and bicycle trail to draw in the community, encourage alternative modes of transportation, provide an attractive yet functional space where access is currently limited or dangerous, and improve the quality of life for its residents.

East Bay Greenway Project



Before



After

Project Milestones

Begin Preliminary Engineering, Environmental Clearance, and Implementation Strategy	September 2010
Perform Environmental Scoping	November 2010
Perform General Environmental Studies	March 2011
Complete Preliminary Engineering Studies	August 2011
Obtain Environmental Clearance	March 2012

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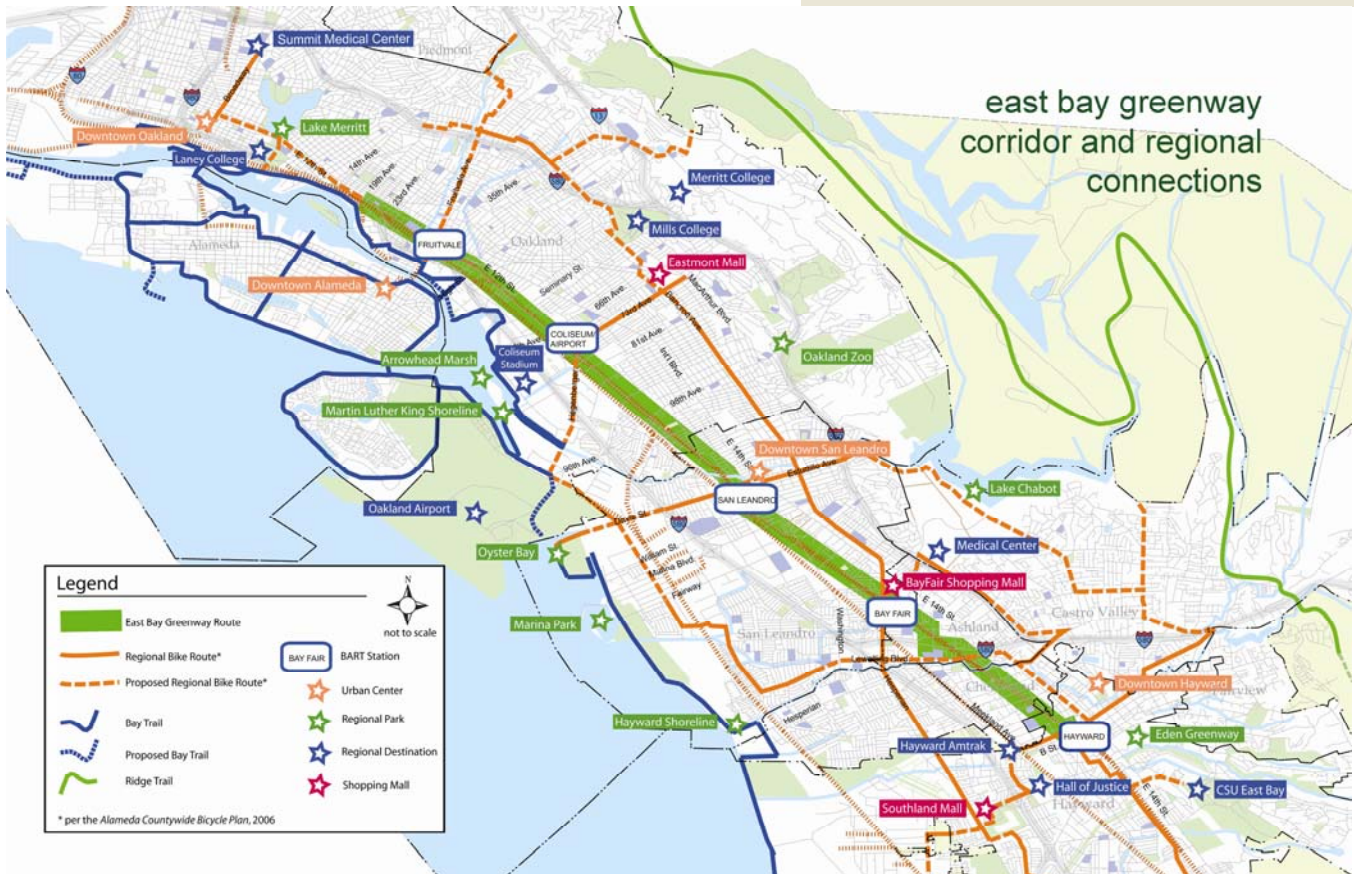
Alameda County's half-cent transportation sales tax, known as Measure B, will generate more than \$3 billion over 20 years for transportation improvements in Alameda County. Over **\$80 million** is dedicated to **bicycle and pedestrian safety and access**. In voting for Measure B, which dedicated 5 percent of the sales tax revenue to bicycle and pedestrian projects and programs, Alameda County residents voiced their support for these time-honored modes of travel.

Measure B has transformed the way bicycle and pedestrian projects are financed in Alameda County by providing each city and the County with a steady, flexible stream of funding for making walking and biking improvements. The funds are being used to maintain facilities, close gaps and construct new facilities such as bike lanes and pedestrian crossings.

The Measure B bicycle and pedestrian funds are distributed in two ways. Seventy-five percent of funds go directly to Alameda County cities and the County based on population. The remaining 25 percent funds countywide programs and projects, including a competitive grant program called the Measure B Bicycle and Pedestrian Countywide Discretionary Fund and a Countywide Bicycle and Pedestrian Coordinator. To date, ACTIA's grant program has funded 41 capital projects, programs and master plans, totaling \$9.5 million, over four grant cycles.

Leveraging Funds

The East Bay Greenway has \$1.4 million in funding, including local funds and an Alameda CTC Countywide Discretionary Fund Cycle 4 Bicycle and Pedestrian Grant, and was included in a TIGER II grant to support project development and delivery activities including environmental clearance for a 2.1-mile section of the Greenway. The project sponsors are continuing to seek additional funding to complete the project through construction.



For more information about the East Bay Greenway project, contact Arun Goel, Alameda CTC Associate Transportation Engineer at (510) 267-6129 or agoel@actia2022.com.



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Funded by Measure B Sales Tax 2002 - 2022

